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TWENTY LOTS IN MANOA VALLEY, formerly Montano's Tract, \$2,500 a lot.

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FIFTY LOTS IN KEKIO TRACT, opposite Makee Island \$600 a lot.

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Etc., Etc.

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RICH LAND —IN— Central Kona FOR SALE

By order of REINNE RODANET, Trustee, I offer for sale those certain parcels of land situate at Onouli, South Kona, Island of Hawaii, described in Royal Patent (Grant) No. 1162 to F. O. Schulze, and containing 174 acres. Royal Patent (Grant) No. 2862 to Awahua and containing 789 75-100 acres, situate at Keopuka and Onouli, Island of Hawaii.

This is a tract of land of over 913 acres, situated in the most fertile and richest portion of the Island of Hawaii. It faces the new Government road, extends to the sea, and is five minutes' walk from Kealahou Bay, by way of the old Government road which runs through the property. Portions of the land are already under cultivation.

This tract is so situated that it is about midway between Kailua and Hookea, and five miles from Napo-poo, three most important ports of the Kona district. It has sufficient elevation, running up to fourteen hundred feet, to be particularly healthful, and its soil is well adapted to the growth of sugar cane, coffee, fruits, dairying, or for the promotion of diversified farming.

Occupants of this land have easy access to several ports for export, and with the advent of the Kona Railroad will find themselves in close communication with the thriving City of Hilo. This, of course, affords a splendid opportunity for the exporting of farm products to the California markets. With the revival of the Kona Sugar Co., considerable portions of this land can be successfully planted to sugar cane.

This is one of the most splendid opportunities for a good investment that has been put upon the market for a considerable period of time.

Further particulars of

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STOWAWAY DIES AT SEA

Overcome by Heat and Coal Dust in Sonoma's Stoke Hole.

A stowaway on the O. S. S. Sonoma, which arrived at Honolulu yesterday from San Francisco, met with a horrible death when the ship was three days out from the Golden Gate. Overcome by heat and coal dust and suffering from a long debauch in 'Frisco, he finally fell on the deck and died, although everything possible was done for him. The unfortunate man, Henry Dever, aged 23, had been in the navy and had got about a good bit of the world, but his attempt to get to the Hawaiian Islands as a stowaway ended in his death.

When Dever was rounded up after the ship had steamed out of 'Frisco he expressed his willingness to work and when taken in charge of the chief engineer was put down below as a coal passer—the worst work that a man can do aboard a ship. The chief engineer told him that if his work was satisfactory he would be put on the ship's articles on arrival at Honolulu.

When on the third day Dever was working in the bunkers, he suddenly became ill. His mates told him to go into the engine room to get air. He did this but was soon brought back by an engineer and members of the crew alleged that a junior engineer put a shovel into his hands and told him to go to work. They also said that one of his mates took the shovel and told him to go to work. The doctor, having now become crazed by the heat, was brought on deck, where his cries were agonizing. Then he was brought down to his bunk, where the doctor gave him every attention. A coal passer was told off to keep charge of him and it was with the greatest difficulty that he was held down. Later he got on deck—the men thought with an idea of jumping overboard. He was again brought below, but slipped on the deck, injured his head and soon died. The doctor gave the cause of his death as heat apoplexy, brought on by excessive drinking. He was buried at sea.

The coal passers apparently became scared because of Dever's death and about a dozen of them were paid off in Honolulu and new men secured to fill their places.

The chief engineer of the Sonoma told an Advertiser reporter that there was no truth in the story that a shovel was put in the sick man's hands by an engineer. He stated that everything possible was done for the man after it was discovered that he was ill. He also stated that many of the men who signed on as coal passers in 'Frisco had no idea of working and only wished to make matters so miserable in the stoke hole as would guarantee their being paid off in Honolulu, where they imagine that volcanoes throw up gold by the wagon load every day of the year, Sundays included.

INVESTIGATED BY BRECKONS

Coal passers from the Sonoma yesterday made charges before United States Attorney Breckons against officers and members of the crew as being responsible for the death of Henry Dever, who died on the trip from San Francisco.

A hearing was had during the day before the United States Attorney and both sides to the controversy were heard. The men alleged to have been responsible for the man's death were said to be Sinclair, Effort and McMillan, but they were exonerated at the hearing. Among the witnesses examined were the chief engineer, Assistant Engineer Galvin, Dr. Bradfield, James Ellis, Otto Cook, W. R. Mains and E. Wise, the last four being fellow workers with Dever. The friends of the deceased claimed that he was discovered stowed away soon after leaving San Francisco and was forced to work passing coal. On the second or third day he became ill and asked to be relieved, but this the officers refused, and compelled him to continue work. A few hours later he was taken from the furnace room a raving maniac and died shortly after. The ship's doctor declared his death due to heat exhaustion. The men alleged also that some little violence was used to compel the man to work.

Other officers and members of the crew denied the story told by the coal passers and say that nothing was done to compel Dever to work.

After a complete investigation United States Attorney Breckons came to the conclusion that there was nothing in the evidence to justify a prosecution.

The American schooner Philippine, Captain Frederickson, has about 300,000 feet of timber to discharge at Alton's wharf and will then go in dock to be cleaned. She will probably get away in a week for Port Townsend.

The American ship George Curtis has nearly completed discharging at Breckon's wharf but may have to wait here some time for sugar.

The American barkentine Planter, C. H. Chase, sails this morning for San Francisco. She takes 2219 bags of sugar as ballast.

The American bark Gerard C. Tobey, Captain Grove, sails from San Francisco with sugar from Railway wharf this morning.

The American schooner Alice Cooke, Captain Penhallow, sails in ballast for Port Townsend on Friday morning.

The American bark Albert, Captain Turner, will load sugar and sail for San Francisco in about two weeks.

SONOMA BRINGS MAIL FROM COAST

She Left for the Colonies Again Late Last Evening.

The O. S. S. Sonoma, Captain Van Oterendorp, arrived off the port at 6 o'clock Wednesday morning from San Francisco. She had 189 sacks of mail and a large cargo for Honolulu which was not discharged in time to permit the ship to get away at the scheduled time, 5 p. m. It was after 9 when she sailed.

Among those who came in by the Sonoma is Mrs. Sarah Jacobus of Los Angeles, who is to visit her daughter, Professor W. T. Brigham of the Bishop Museum was a passenger. Mrs. George R. Carter, two children and maid, returned from a trip to the Coast.

Mr. and Mrs. P. C. Jones return from an extended tour of the States, leaving Miss Jones there for a longer visit. Mr. Edgar Capless returned from a Home Rule junketing trip to Washington, D. C.

Among the passengers for Sydney is Mr. Franz Wileczek, a well known violinist.

Considerable amusement was created about 5 o'clock when the police pulled up on the wharf with the patrol wagon in which were the two men who stowed away at Auckland on the Ventura. Both men are Austrians and of a happy-go-lucky character. They seemed to enjoy the situation. They carried their baggage up the gangway smiling but later when they were led into the fore'sle and chained to the stanchions their merriment disappeared. They would have to remain in irons until the ship got outside and would then have to work. On arrival at Auckland they will probably be prosecuted and receive a short term of imprisonment.

Mrs. Archie Little and Miss Little, wife and daughter of the chief engineer of the Sonoma, were passengers to Honolulu and will remain here on a visit for a short time.

A trial was to have taken place in San Francisco on July 4th of the Mariposa, which has been equipped for oil burning. She has a capacity for 6500 barrels, sufficient to take her to Tahiti and back without additional fuel. With her new equipment she will make 17 knots per hour.

Captain Bert Williams, of the American-Hawaiian Steamship Company, Oregonian, and late of the four-masted ship Frederick Billings, has been appointed a pilot in San Francisco.

The barkentine Coronado for Honolulu and the schooner H. C. Wright for Maui, sailed from San Francisco on June 29.

The schooner Columbia, from Honolulu, arrived at Port Townsend on June 29.

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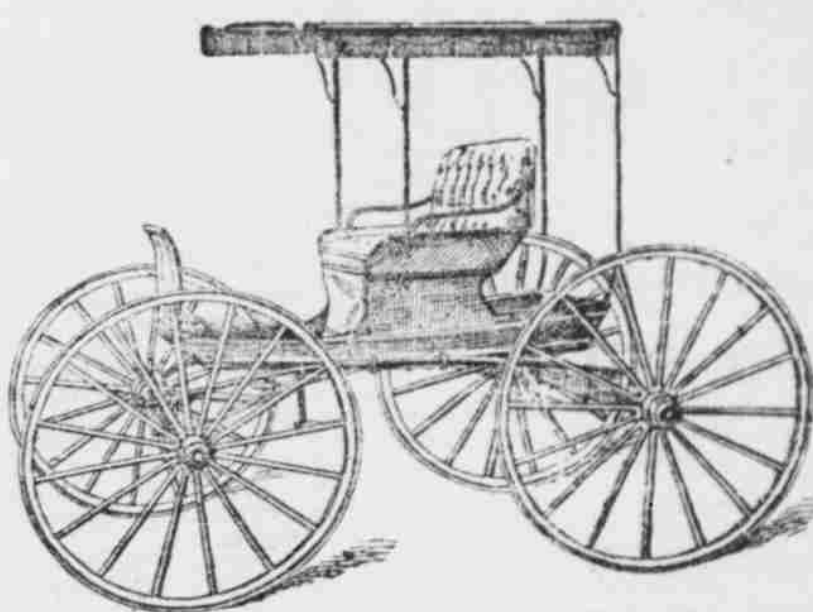
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